

GATEWAY WEST

Introduction

The Gateway West district represents one of the two main entries into downtown Monett. A new roundabout and gateway arch signify the arrival of the downtown area from the west. The west downtown area encourages healthy life styles, active living, and social interaction. This area should contain pocket parks, restaurants, a farmer’s market pavilion, community garden, and a wellness center, as well as the existing commercial businesses that already inhabit this section of the downtown. Greenway trails are used to connect these unique amenities and further emphasize the walkability and healthy living of the downtown.



Figure 6.1.1 Gateway West Site Plan

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Road Alignment



Line of Sight

Visibility of downtown from Highway 37 is weak due to the alignment of the highway with Broadway Street. It is easy to miss the turn onto Broadway Street unless the driver knows it is there. To provide a stronger visual connection of the downtown from Highway 37, we propose the brief east-west axis of Highway 37 be aligned with Broadway Street so that they may terminate into one another. The alignment would require some businesses and home owners to relocate. Trees would be planted along the newly aligned highway to soften the streetscape and provide a uniform periphery so that focus would be given to the intersection with Broadway up ahead.



Figure 6.1.3 Current view traveling east on Highway 37 towards Broadway Street



Figure 6.1.2 Current view traveling south on Highway 37 towards Broadway Street



Figure 6.1.4 Current view traveling west on Broadway Street to Highway 37

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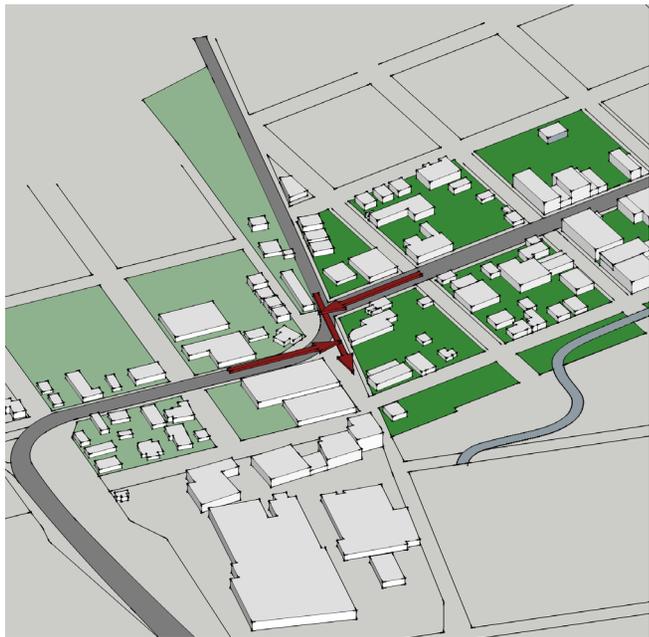


Figure 6.1.5 Diagram of the current view situation



Figure 6.1.6 Diagram of views after proposed roundabout showing a complete axis between Highway 37 and Broadway Street

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Roundabout



Safety

At the intersection, we propose implementing a roundabout rather than a traffic light with right angles. The roundabout will give visitors a grand sense of arrival and will reduce the speed of traffic as drivers approach causing them to be more aware of their surroundings. A roundabout ensures a safer and more efficient means of traffic flow. According to a study done by the Federal Highway Administration, vehicle collisions are reduced by at least 40 percent and fatal injuries by 90%. The same study also showed that roundabouts can save a city up to \$5000 in electricity and maintenance costs of traffic lights. Roundabouts also reduce carbon emission and fuel consumption for drivers because vehicles aren't idle on the intersection.



Figure 6.1.8 Aerial of proposed roundabout at Highway 37 and Broadway Street



Figure 6.1.7 Cordata Parkway Roundabout in Bellingham, Washington



Figure 6.1.9 Diagram showing traffic flow after the propose roundabout at Highway 37 and Broadway Street

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Aesthetic

The roundabout on Highway 37 will be wide enough to accommodate vehicles with a large turning radius. Raised splitter islands will be introduced along the roadway of entrances and exits to better define the flow of traffic. They will also improve the safety of pedestrian crossing by providing a space to briefly wait until it is clear to continue crossing on the crosswalks. Within the island of the roundabout, there should be a fountain and landscaping to enhance the streetscape and serve as a grand entrance and/or important intersection. along Highway 37.



Figure 6.1.10 Proposal of the roundabout fountain with three people looking at a map, aiding one another to represent the close bonds Monett citizens have to help one another



Figure 6.1.11 Rendering of the roundabout at night illuminated by lighting



Figure 6.1.12 Rendering of the roundabout from highway 37

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Gateway Arch



Signage

A gateway arch will be prominently placed at the entrance of Broadway to help signal the arrival of visitors and residents alike. Special attention and care will be given to the design so it is cohesive with the character of the downtown. The signage will incorporate materials that are commonly used in the surrounding site, so the columns will have a brick finish but the arch will be of steel for structural integrity. The letterings will be constructed out of steel and stick out to give it more character and visibility from a distance. The steel will be dark green to match the light poles that are currently in downtown and this is so the signage compliments not just the buildings but the streetscape also. The gateway arch will have a variety of color to ensure that it stands out more and creates an impression on people. Along the ground, there will be a wide variety of vegetation such as evergreen shrubs, flowers, and trees to not only provide a sense of scale and place, but help strengthen the entry visually. Trees will also be placed to create a backdrop for visitors coming in. By placing a well-designed signage illustrating the qualities of what the community stands for, visitors can quickly have an understanding of what Monett represents and has to offer as a city.



Figure 6.1.13 Image of signage to mark the arrival of a new district



Figure 6.1.14 Rendering of proposed gateway arch for downtown Monett

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Street Beautification

Highway 37

The streetscape on Highway 37 just west of Broadway lacks a sense of invitation. The current state of the buildings along the highway is unkempt and the lack of green space detracts from the entrance to downtown Monett. We recommend the realignment of Highway 37 with Broadway Street. This alignment would create opportunities for trees and vegetation to be integrated along the road. The trees and vegetation will act as a buffer zone between the highway and businesses and create a better sense of welcome to downtown Monett. The fresh, calming and attractive green space along the highway should blend with the city center streetscape and City Center Park and draw the visitor into the “natural landscape” of downtown.



Figure 6.1.15 Looking down highway 37



Figure 6.1.16 Image of signage to mark the arrival of a new district



Figure 6.1.17 Pedestrian/bike trail that runs through highway 37

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Regional/Local Community Interaction



Restaurants

Around Broadway and 1st streets, we recommend renovating the existing structures and establish commercial retail stores and restaurants. These new businesses should have entrances in both the front as well as the back. The buildings should have brick facades to remain cohesive with the rest of the downtown. The restaurants should be located on the corners of the block to soften the streetscape and allow for seating to flow out from the restaurants onto the sidewalks. The restaurants could have a partnership with the nearby farmers market and community garden and serve locally grown produce. We propose creating a new parking lot just behind these new businesses to provide convenience to the new establishments, the smaller Market Park, City Center Park, as well as the rest of the downtown area.



Figure 6.1.18 Building on the corner of Broadway and 1st St



Figure 6.1.19 Storefront facade for the back side of the restaurants with outdoor eating area



Figure 6.1.20 Rendering of proposed restaurants for downtown Monett

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Social Focus

Wellness Center

The vacant Ramey’s Supermarket at the corner of Central and Highway 37 should be renovated to bring activity to the west entrance of downtown Monett. This location at the interface between the regional and local communities has the potential to energize this important entrance to Monett and to strengthen the physical and social connections within the broader community.

We believe that the size and location of this facility at the gateway into Monett makes it suitable for a community wellness center. Such a center would complement the nearby YMCA, farmer’s market, and community gardens activities and also link directly to the greenway trails for access and outdoor exercise activities. We envision a center that fosters wellness and sustainable lifestyles and sponsors classes and activities such as: Yoga, Tai Chi, Pilates, aerobics, dieting, healthy cooking, gardening, etc.



Figure 6.1.21 Ramey’s market along Highway 37



Figure 6.1.22 Indoor and outdoor spaces for fitness use



Figure 6.1.23 Night rendering of the wellness center

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Farmer's Market



Farmer's Market

Farmer's markets promote community development by introducing a public gathering space for local farmers to sell and trade agricultural produce. Social interaction is encouraged between the local farmers and the public. A sense of local pride and economic development would arise due to the community buying locally grown produce; with the revenue staying in Monett. To further promote the farmer's market music, entertainment, and group activities should be provided.

A pavilion that will be able to hold not only the farmer's market, but other community activities, will be placed at the west end of the City Center Park. It will have three entrances, one from Broadway Street, 1st Street, and the City Center Park trail. This location provides easy for farmers transporting large amounts of produce or plants. Farmers can temporarily park along Broadway or 1st Street to drop off their produce and then move their vehicles to a nearby parking lot between 1st and 2nd Street. The farmers can also temporarily park in the lot located north of the pavilion, next to the newly constructed restaurants. The open air pavilion should be a post and beam wood construction. Lighting should be placed along the posts and bathroom facilities should be located near one of the entrances. The pavilion contains seating areas for small and large groups and has a plaza space for more outdoor activities like picnics and barbeques.



Figure 6.1.24 Plaza space for additional activities



Figure 6.1.25 Rendering of proposed market pavilion

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Farmer's Market

Community Garden

We recommend placing a community garden next to the farmer's market to stimulate social interaction and encourage self-reliance. The community garden will be located west of the market pavilion and will only be accessible from the market pavilion. The community garden is not fenced, but a 2 ½ ft tall wall is proposed to act as a physical and psychological barrier. We understand that an enclosing fence would keep trespassers out, but we believe that it would send the wrong message to the community. If there is ever a need for future expansion, an area on the east side has been left open to allow the garden to expand eastward towards the City Center Park. In addition, trees should surround the garden creating necessary shade for some of the plants. The garden will also contain benches for relaxing. A community garden is a great asset to the community. Studies have even shown that communities with community gardens improved social relations resulting in a decrease in crime and vandalism.



Figure 6.1.26 Garden connected to market pavilion



Figure 6.1.27 Physical and psychological barrier instead of fenced

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Farmer's Market



Community Garden



Figure 6.1.28 Fremont Community Garden Sacramento, CA



Figure 6.1.29 Strawberries



Figure 6.1.31 Girl enjoying a piece of vegetable



Figure 6.1.30 Radishes



Figure 6.1.31 Community garden



Figure 6.1.32 Community Garden Seattle, WA

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Streetscape

Lighting

The lighting in downtown must address the pedestrian and vehicular traffic and signage needs. We recommend that the fixtures along the sidewalk be multifunctional. They should be able to offer illumination to both the vehicles and pedestrian and brackets for banners and way finding signage. A well planned lighting design can improve the safety of the area and help enhance the downtown by attracting visitors, encourage evening activities and creates a strong identity. Banners hung throughout the downtown help promote community or seasonal events and strengthen the festiveness of downtown.

The recently installed light fixtures provide good illumination, accommodate banners and way finding signage and compliment the character of downtown. However, if the city decides to widen the sidewalk, the light fixtures will need to be moved, or replaced. If new light fixtures are added they should also complement the downtown's historical or traditional look, like the current fixtures.



Figure 6.1.33 Multifunctional lamppost



Figure 6.1.34 Lamppost with hung flower pot



Figure 6.1.35 Ornamental Lamp post

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Streetscape



Vegetation

The Gateway West vegetation should match the plantings in the City Center and Gateway East section of downtown. Special attention should also be given to the aesthetics appeal and maintenance requirement of this vegetation. We suggest a mix of evergreen shrubs and trees. Evergreens are able to keep their leaves throughout the year. However, they should be planted no later than mid-august in order for the root system to obtain plenty of internal moisture before the ground freezes. For trees, slower growing types are recommended because they require less maintenance and they have enough strength to withstand harsh weather. Drought resistant trees such as oak, pine, willow, elm, hickory, and maple should be considered. We would also recommend such shrubs as Fuchsia, Hollyleaf Cherry, Bigberry Manzanita, Buckwheat, and flowering plants like Desert Willow, Beach Suncups, Red Buckwheat and Golden and Woolly Yarrow.

Streetscape Trees Benefits:

- Promotes emotional and physical health
- Draws new businesses to the area
- Attracts visitors and encourages them to stay longer
- Improves air quality in the area
- Reduces noise, glare, and ambient temperature
- Increase marketability of properties
- Reduces utility cost by providing shade for buildings



Figure 6.1.36 Render of streetscape along boulevard



Figure 6.1.37 Trees help make the downtown more attractive

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Streetscape, Facades, Pocket Parks, and Parking

Crosswalk Accessibility

Currently there is a lack of crosswalks in the downtown area towards Highway 37. We propose the implementation of crosswalks to make downtown more pedestrian friendly. The crosswalks could then be raised about to slow traffic down. Pedestrians could then easily walk across these “speed bumps” safely. Crosswalks could be created using striping or a different texture material from the street. Various materials could be chosen such as stamped concrete or brick. Some brick options could allow for citizens or organizations within the community pay for a brick and have their name imprinted on it. This allows for personalization within the actual street.



Figure 6.1.38 Crosswalk Madrid, Spain

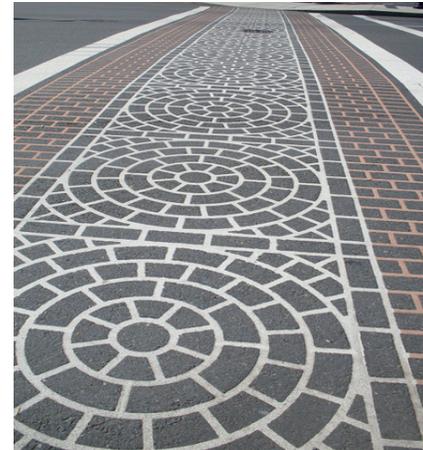


Figure 6.1.39 Patterned crosswalk



Figure 6.1.40 Textured crosswalk

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Facade Study



Facade Study

Many of the buildings along Broadway Street have been clad with materials that cover their original façade and distract from their appearance and weakens the cohesive relationship of the buildings along the street. We recommend removing the cladding and revealing the original material. Removing the cladding will enhance the aesthetics appeal of downtown. Additionally, many of the windows on the upper stories of the buildings in downtown Monett are covered up; this conveys a sense of economic distress. We recommend opening up the upper floor windows and sidewalk storefronts to promote a sense of social and economic vitality. This would be more welcoming to the visitors to Monett. We would also recommend the addition of awnings to create a streetscape that is more comfortable for pedestrians; these awnings would provide shade, scale and interest.



Figure 6.1.41 Current north facing facade 2nd to 3rd



Figure 6.1.42 Perspective of south facing facade 2nd to 3rd



Figure 6.1.43 Storefront facade gives a sense of welcoming

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Facade Study



North facing facade 2nd to 3rd st -Before



North facing facade 2nd to 3rd st -After

Figure 6.1.44 Facade Study before and after

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Facade Study



South facing facade 2nd to 3rd st -Before



South facing facade 2nd to 3rd st -After

Figure 6.1.45 Facade Study Before and After

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Pocket Parks

Pocket parks are open spaces within an urban area that can be utilized for almost any use. The green spaces in the Gateway West area will become pocket parks that will provide a network of connections from Bond Street to the City Center Park. The pocket park located to the north of Broadway between 2nd and 3rd streets contains a space sloping upward that could create interesting seating areas. We propose each seating area to be designed relating to the sloped condition it is placed upon. Trees and sculptures should be placed to help soften the hard surface materials in the downtown area. The pocket park to the south is more private since the space slopes away from the street and is basically enclosed on 3 sides. It should serve more as an entrance into City Center Park.



Figure 6.1.46 Location of proposed pocket park



Figure 6.1.47 Render of proposed pocket park within Gateway West



Figure 6.1.48 Seating area created within the slope of the green space

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Market Park

Currently the auto mechanic shop that is on the corner of Highway 37 and Broadway Street weakens the image of the downtown area. We recommend placing a small park southeast of the new roundabout to serve as a temporary stop for visitors to relax and play before continuing towards the linear City Center Park. The smaller park should contain seating as well as a playground for kids. We also suggest establishing a reflective pool to create a serene atmosphere in conjunction with the active playground. The seating, playground, and pool should be located on the corners of the park to create an open space in the center for activities such as Frisbee throwing, playing catch, tossing a football, or even playing Red Rover. The park should be slightly recessed into the ground to create visibility, yet more privacy to the Highway 37 and Broadway interchange. We also propose trees and vegetation be planted for additional privacy and shade.



Figure 6.1.50 Rendering of proposed market park



Figure 6.1.49 Area of where market park would go.



Figure 6.1.51 Open green space for activities such as barbecuing or playing catch

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Parking

Currently, parking in Gateway West is angled. We propose converting angled parking into parallel parking. Studies have shown that parallel parking is much safer. In an article published by the Institute of Transportation Engineers, it has been shown drivers leaving the space have limited visibility to the rear, and drivers involving lane change can cause sideswipe and rear – end collisions. Research collected also show switching from angled parking to parallel parking reduces crash rates by 50 to 70 percent.

Although the number of parking spots is reduced along Broadway Street, we propose creating new parking lots. There will be one located on Bond Street and one near City Center Park. These areas are currently graveled lots but we suggest a more hard surfaced permeable material. There will also be a newly built parking lot between the proposed restaurants and the market pavilion.

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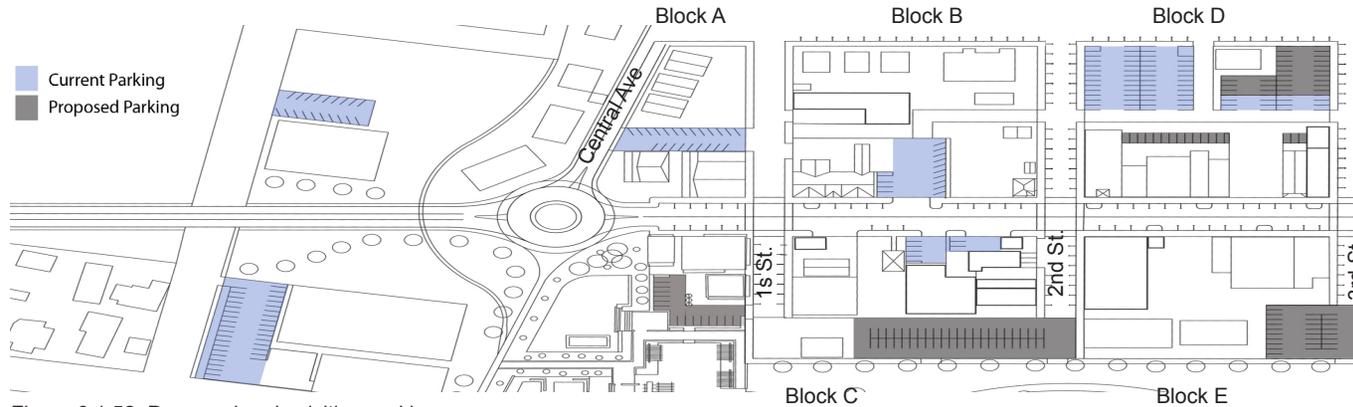


Figure 6.1.52 Proposed and existing parking

Parking Spaces per Block	Block A	Block B	Block C	Block D	Block E	TOTAL
Existing On Street	0	14	6	15	18	53
Existing Off Street	20	13	8	61	0	102
Proposed On Street	5	36	21	49	25	136
Proposed Off Street	0	0	44	44	25	113
Difference	-15	+9	+51	+17	+32	94

Figure 6.1.53 Parking Spaces in each block

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The City Center is located in the heart of the downtown area and is home to the majority of the city center businesses. The City Hall and other prominent facilities that pertain to the city of Monett are also in this corridor. The City Center should be easily walkable and promote commerce between the businesses, visitors, and the citizens of Monett. The widening of sidewalks, increased signage, and larger accommodation for parking all help to increase traffic flow into the stores located in this district. The street edge should be maintained and stores should have seamless transitions between the sidewalk and their stores to encourage business from people just walking past. The City Center Park just south of the City Center flows up to Broadway through pocket parks and a grand entrance into the park is on the same axis as the City Hall to provide a visual connection between the existing city of Monett and the new social park.



Figure 6.2.1 City Center Site Plan

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The Streetscape

Parking

Currently the parking situation in the downtown urban core consists of angled parking on Broadway Street with several parking lots located throughout the blocks between Broadway and Bond streets. We recommend converting the angled parking to parallel and increase the number of parking spaces on 3rd, 4th, 5th, 6th, 7th, 8th, and Bond streets. Many of the existing parking lots can be reworked to accommodate more parking spaces as well.

- Research studies have shown that the risk of vehicular crashes decrease anywhere from 50% to 70% by switching to parallel from angled parking.
- Parallel parking also aids in pedestrian safety because it slows traffic speed when parking or exiting a spot.



Figure 6.2.2 Angled parking in downtown Monett



Figure 6.2.3 Image of parallel parking

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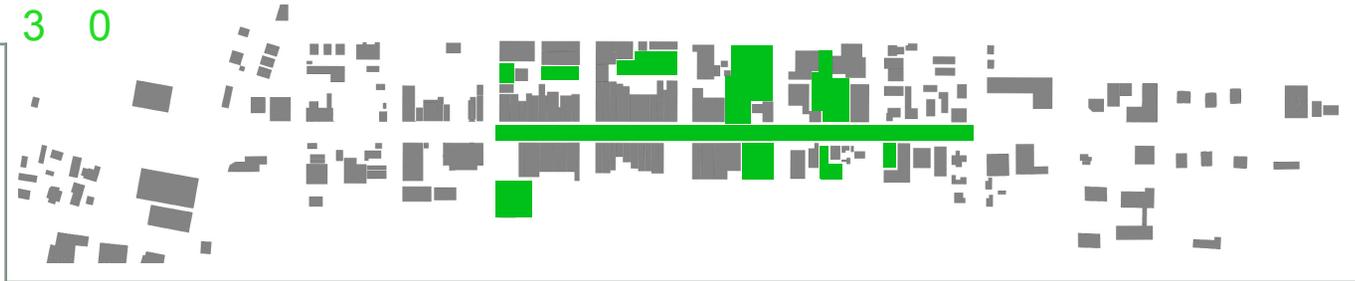
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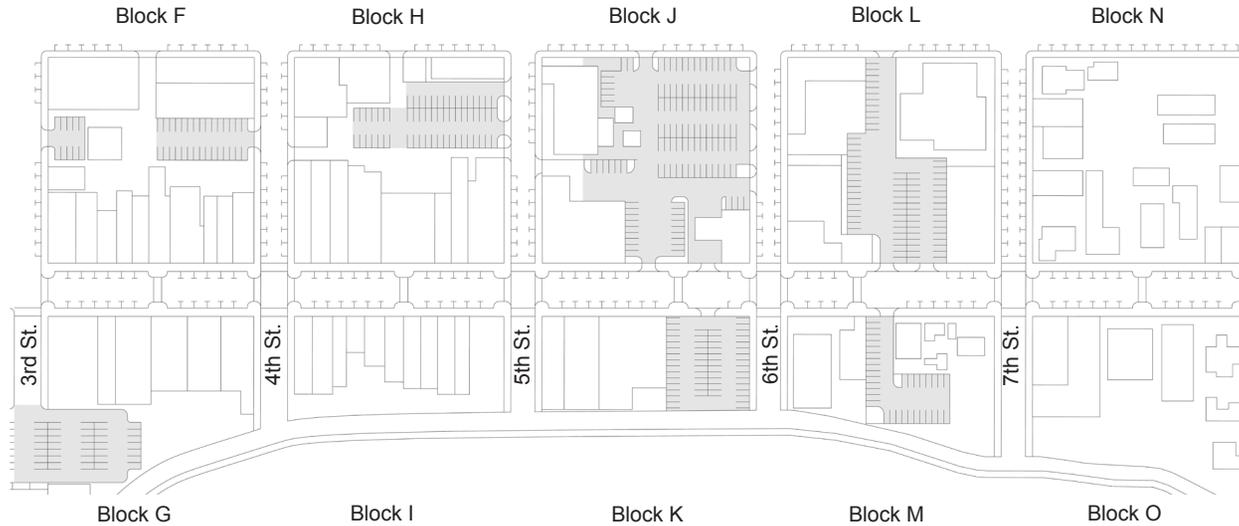


Figure 6.2.4 Proposed Parking Condition

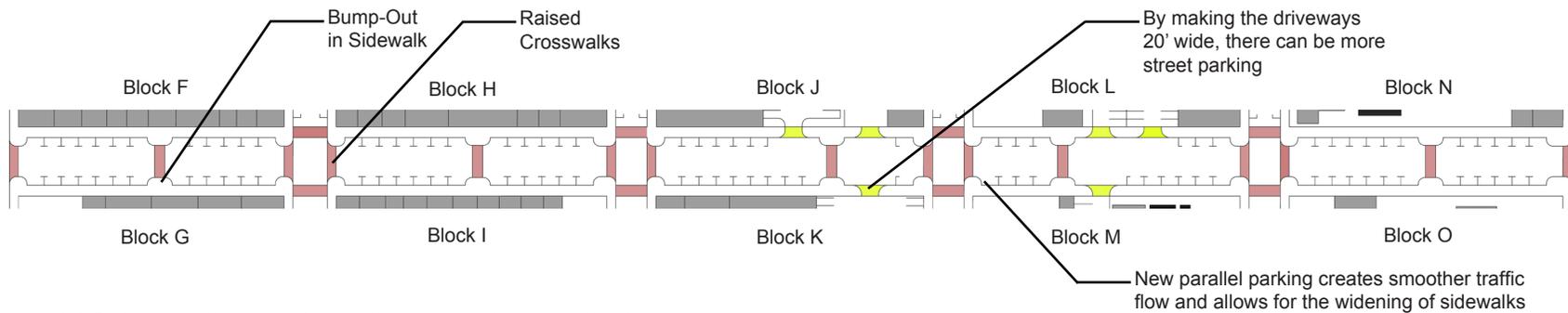


Figure 6.2.5 Proposed On-Street Parking Condition for Broadway Street

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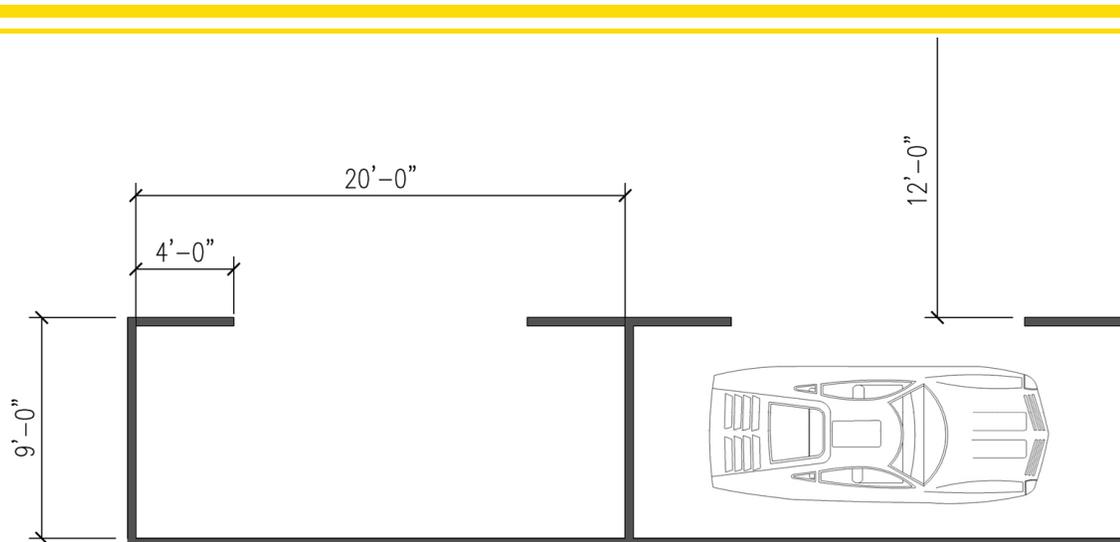


Figure 6.2.6 Parallel Parking

Parking Spaces per Block	Block F	Block G	Block H	Block I	Block J	Block K	Block L	Block M	Block N	Block O	Block X	TOTAL
Existing On Street	18	15	25	15	11	10	9	7	13	16	0	139
Existing Off Street	23	41	56	0	88	27	45	25	0	0	15	305
Proposed On Street	45	13	42	13	34	10	46	10	56	13	0	282
Proposed Off Street	40	35	57	0	127	52	76	34	0	0	0	421
Difference	+54	-8	+18	-2	+62	+25	+68	+12	+43	-3	-15	+244

Figure 6.2.7 Proposed Parking Calculation Chart - Refer to iagram on page 100

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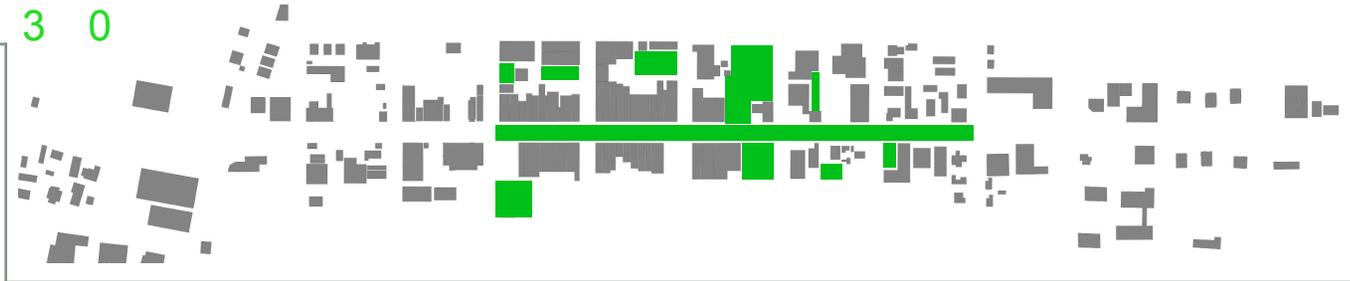
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Parking

In this new parking scheme, the distance a visitor must travel from a parking space to their destination store is less, and the journey is more comfortable and enjoyable, than what people experience when they walk from their parking space at Wal-Mart to the store.



Figure 3.2.8 Image of downtown Monett



Figure 6.2.9 Image of a vast Wal-Mart parking lot



Figure 6.2.10 Image of parking lot at Wal-Mart

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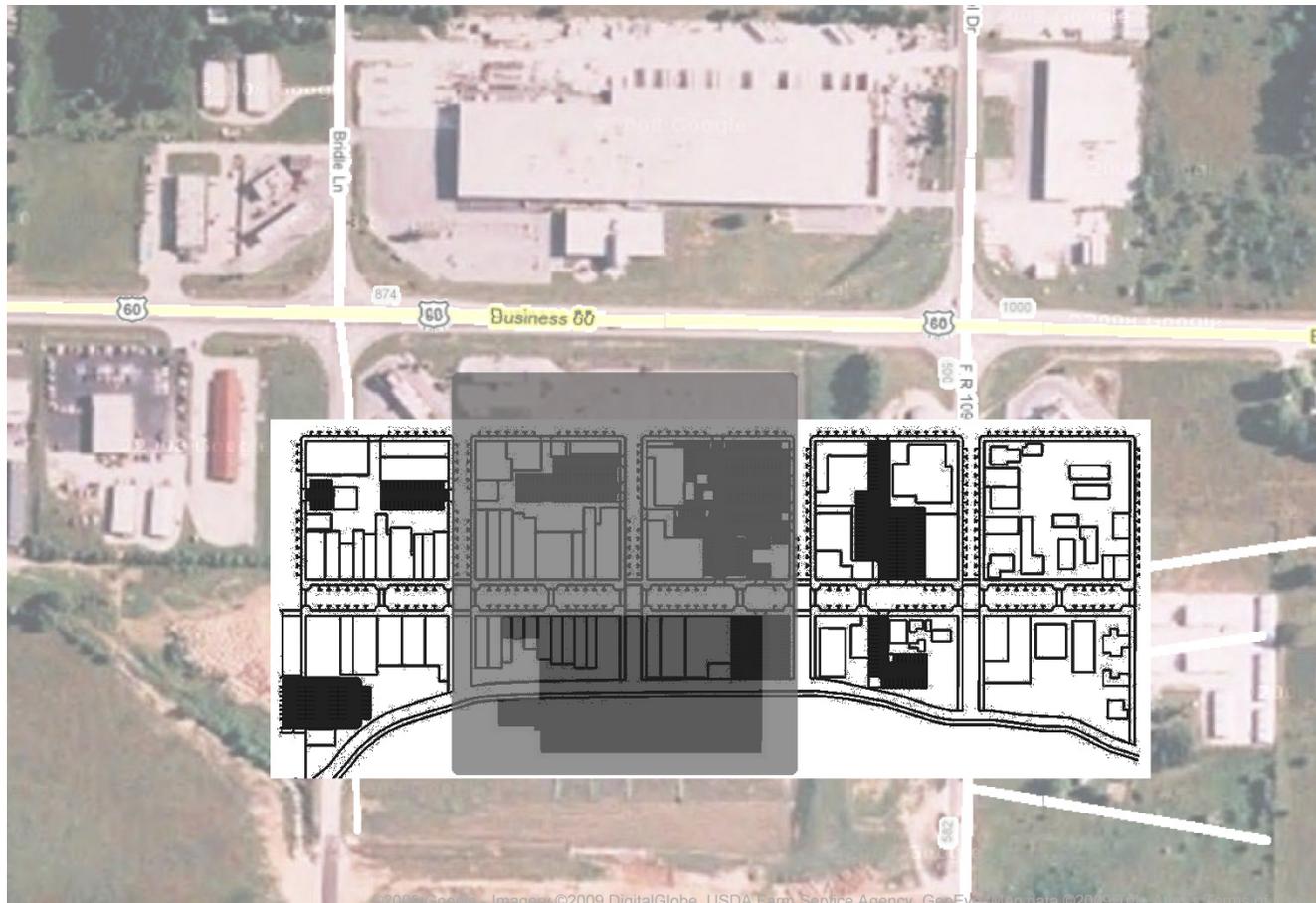
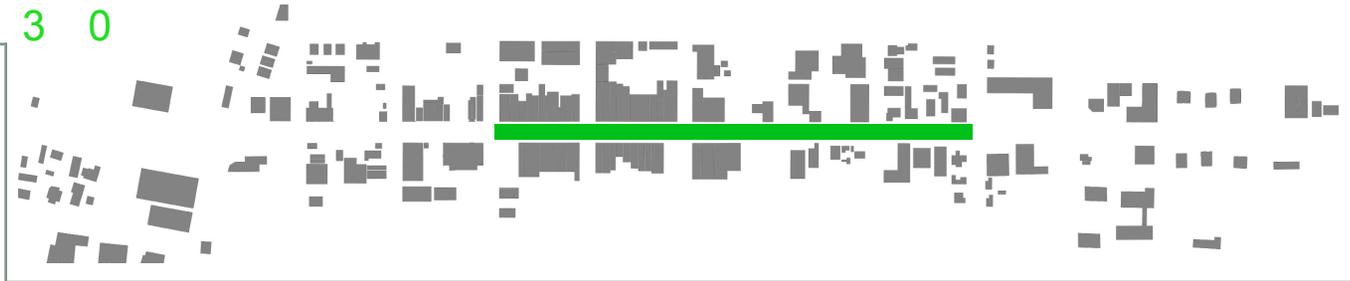


Figure 6.2.11 Diagram showing the edges of the Wal-Mart parking lot and building over the downtown area. People easily walk a distance more than that of two city blocks to reach the Wal-Mart store. There is ample parking within the City Center district to reduce the walking distance from car to store.

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Sidewalks

Sidewalks should be full of life with pedestrian traffic. In order to act as a vibrant transportation path through the downtown area, the sidewalks must be wide enough to accommodate people walking and talking alongside one another as well as people coming from the opposite direction. Café’s and businesses should also be able to flow out onto the sidewalk to eagerly welcome customers. The sidewalks on Broadway Street should be twelve feet wide with an additional six foot wide, functional and aesthetic barrier separating the sidewalks from the street. Within this barrier, lighting, vegetation, and way-finding signage would be used to soften the streetscape. The sidewalks on the south side of Bond Street should also be widened to promote this same dynamic atmosphere and increase the pedestrian traffic to the businesses located just north of Broadway Street. The north side of Bond Street should be maintained in order to respect the existing residential zone just north of the downtown area.



Figure 6.2.14 unique paving done with different colored pavers

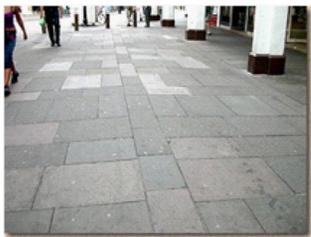


Figure 6.2.12 concrete paving

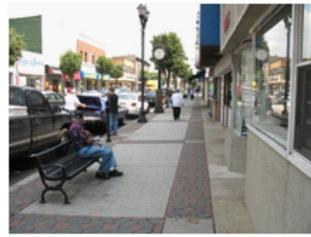


Figure 6.2.13 combination of poured concrete and pavers



Figure 6.2.15 The sidewalk and crosswalk materials should be different to differentiate the sidewalk from the street zone

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Nodes

Currently the existing sidewalk is one continuous path between streets without any places for social gatherings. We recommend sidewalk nodes that bump out into the street the same width as the parallel parking. By extending the sidewalk out in between on-street parking spots, it helps to better define the parallel parking as well as create unique larger nodes within the sidewalk space that promote small group social gathering. Extra seating should be placed here to offer a place of rest and socialize while out shopping. These nodes can also be used to shorten the distance of crosswalks. Nodes should also be established midblock to promote access across the street without having to walk to the end of the block. Special attention should also be made to the texture of the sidewalk as a transition cue from the node to the crosswalk. By implementing a new texture, the visually impaired would be able to recognize when the sidewalk ends and the crosswalk begins. All crosswalks should be slightly raised to create a visual and textural cue for the motorist and to ensure pedestrian safety.



Figure 6.2.16 Image of nodes with vegetation

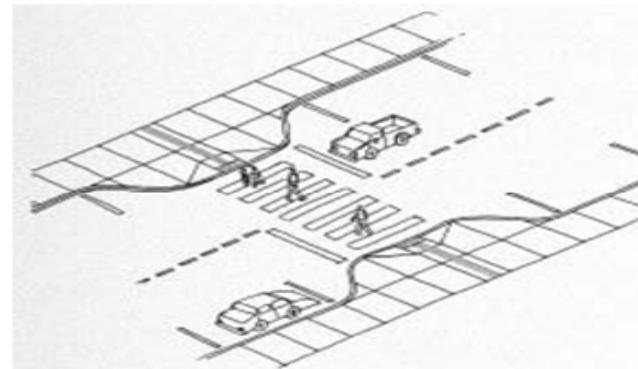


Figure 6.2.17 Nodes with crosswalks that define parallel parking and allow for extension of the sidewalk

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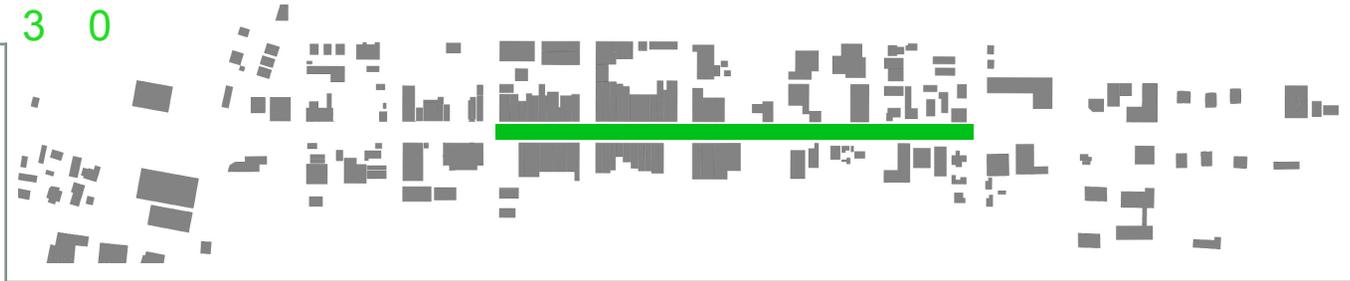
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The Streetscape



Lighting

Monett should have a wide variety of lighting types to illuminate the City Center area for safety, signage, and aesthetics. The street should be well lit at night to allow drivers to see the boundaries in which they must stay as well as pedestrians that may be crossing the street on the crosswalks. These fixtures should have direct down-lighting so as not to shine on the upper level lofts. A different style of lighting should address the sidewalk to give a unique atmosphere at night. Lighting should be indirect to give an overall glow with small accent lights on the vegetation to give a completely different feel to the streetscape at night. Signage should be lit to promote businesses and promotional advertising for both the street and the sidewalk. We recommend using a lighting type that has multiple functions. The street and sidewalk lighting should be located within the same fixture to reduce the number of lighting poles needed. Each of these lighting fixtures should also allow for a banner to be hung publicizing events and festivities Monett may host.



Figure 6.2.18 This depicts how proper lighting can promote a dynamic night life.



Figure 6.2.19 Render of light fixture in the downtown area

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Figure 6.2.20 Lighting Diagram



Figure 6.2.21
Light Fixture with Signage



Figure 6.2.22
Single Fixture with street
and pedestrian lighting



Figure 6.2.23
Decorative Fixture

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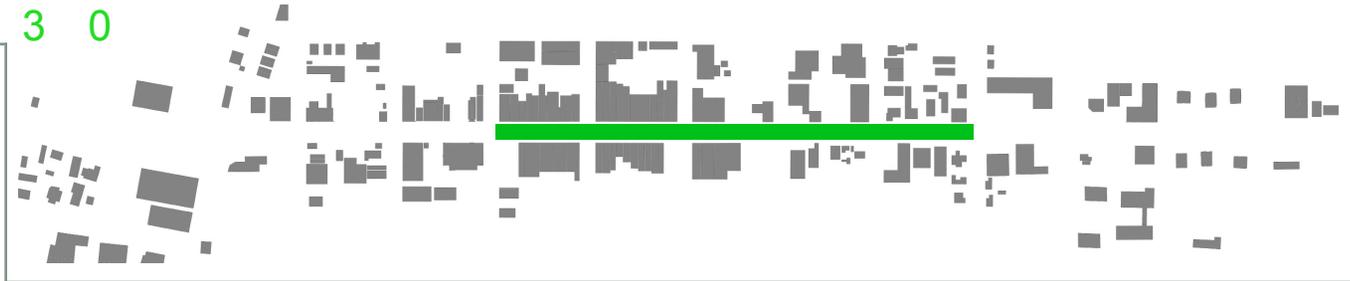
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Signage

Vehicular traffic and pedestrian traffic have different viewpoints from one another. Signage should then respond to the vehicles driving by on the street and the pedestrians walking along the sidewalk underneath buildings' awnings.

The signage for vehicular traffic should be placed above awnings within a uniform horizontal band to offer cohesion between all of the buildings in the urban core district. The different types of signage should promote the business or office as intended, but it should still reflect the character and placement of the City Center streetscape.

The pedestrian signage should consist of smaller hanging signs that protrude out from the façades or down from the awnings. This would allow people on the sidewalk to clearly see the names of businesses before actually encountering the store. Signage could also occur on the sidewalk or in the windows for store promotions.

Wayfinding signage should be dispersed throughout the downtown area to give visitors a sense of location and help direct them to different attractions.



Figure 6.2.24 Rendering of store signage above awnings for vehicular traffic, hanging signage underneath awnings for pedestrian traffic, and banners for advertising of city events.



Figure 6.2.25 Rendering of wayfinding signage at the corner

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Wayfinding Signage

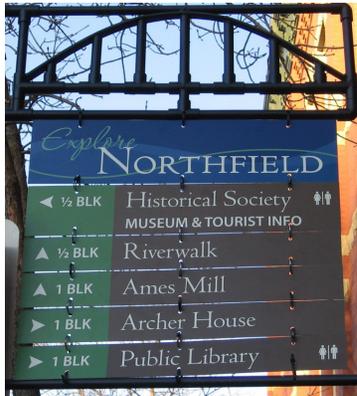


Figure 6.2.26
Directional Signage



Figure 6.2.27
Definition of bike traffic



Figure 6.2.28
Map of downtown

Store Signage



Figure 6.2.29
Signage above awning



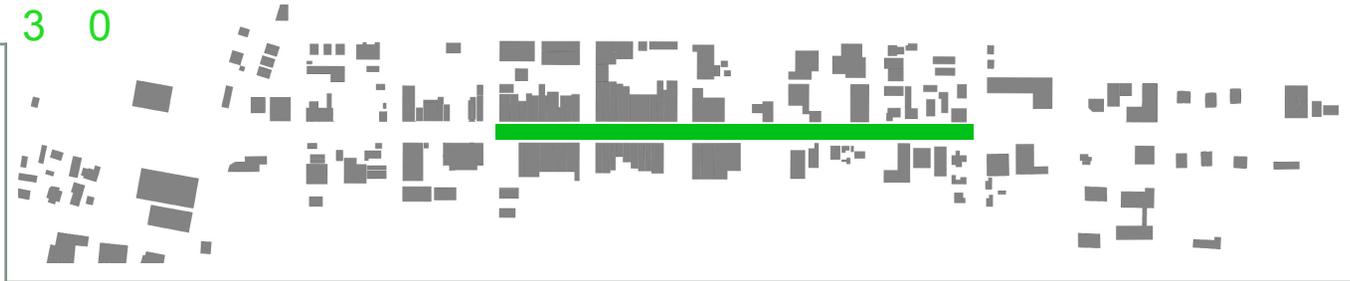
Figure 6.2.30
Decorative signage over awning



Figure 6.2.31
Signage reflecting the character of the building

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The Streetscape



Vegetation

The streetscape should include a variety of vegetation to provide shade, human scale, spatial definition, storm water drainage, and to promote the overall aesthetic of the downtown. Vegetation should be used to anchor and define parking spaces within established parking lots as well as located in the six foot barrier between the sidewalk and the on-street parking. Within this barrier, careful selection of vegetation to create bioswales should be made. Bioswales should be established to help collect and drain storm water run-off. The bioswales then clean and filter the water as it drains into the soil, eventually replenishing the water table beneath. By placing trees on the other side of the sidewalk than the building facades, a pedestrian boulevard is created which makes the character of the downtown much more enjoyable to walk amongst.

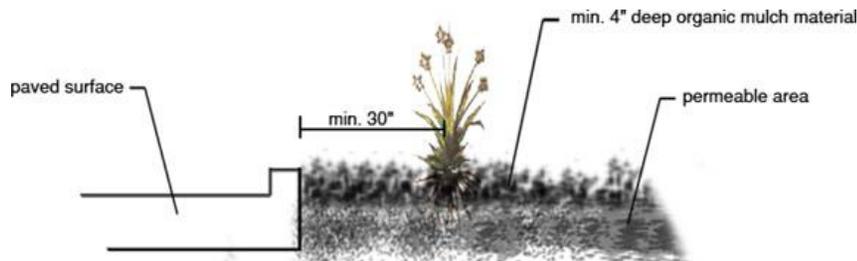


Figure 6.2.32 This shows the minimum distance needed between the street parking and actual plants in the bioswale.

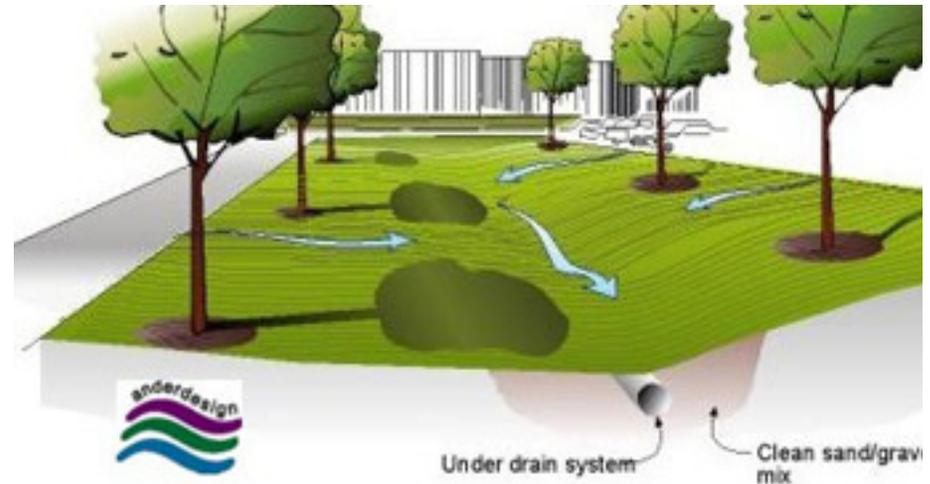


Figure 6.2.33 This diagram shows how bioswales collect storm water

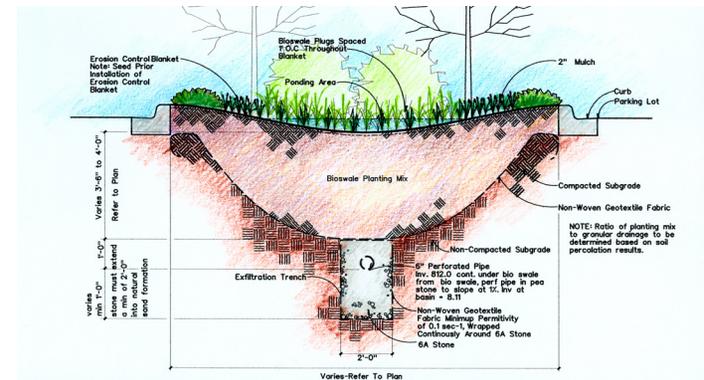


Figure 6.2.34 This diagram shows the construction of a bioswale and the drain underneath that collects the storm water.

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Seating

Seating directly affects the social atmosphere of a streetscape. Although sidewalks primarily function as a pedestrian transportation route, they should also focus on the social needs of those using the sidewalk. Seating provides a place for the pedestrian to slow down and enjoy other’s company or simply watch people as they carry on their way. The seating primarily occurs within the nodes where there is more space to gather. The seating arrangements should be placed in such a way that encourages social interaction. Some of the planting boxes should be designed wide enough to offer additional seating. Besides locations within the nodes, benches could be placed in front of businesses underneath the awnings where people can observe the street life and casually wait for a friend or family member. Seating should be located within pocket parks as a place to rest and socialize within a softer landscape.



Figure 6.2.35 Seating



Figure 6.2.36 Seating

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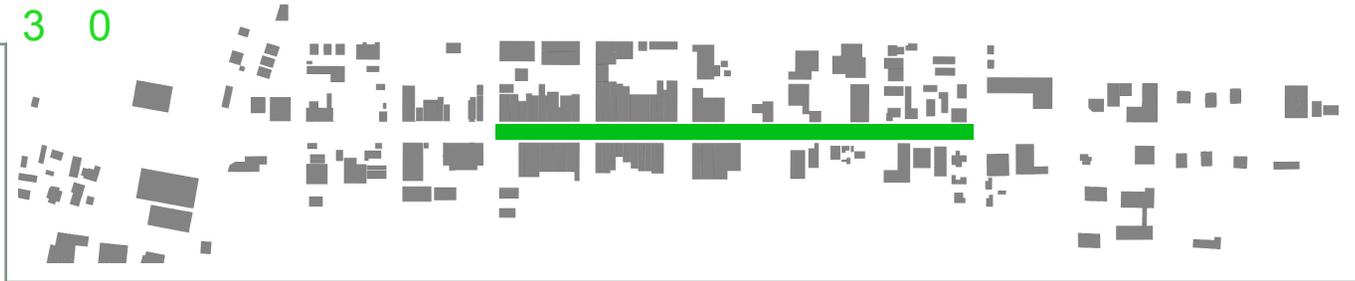
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The Streetscape



Seating



Figure 6.2.37 Seating



Figure 6.2.38 Seating



Figure 6.2.39 Seating



Figure 6.2.40 Seating

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Waste Management

The downtown urban core should address the increase waste that comes with the increase in vehicular and pedestrian traffic. Trash receptacles should be placed along the sidewalks in a consistent manner to prevent the generated trash from littering the sidewalks. Recycling canisters should also be utilized to promote a more sustainable way of living. All waste receptacles should physically blend with the character of the urban core.



Figure 6.2.41 Decorative Trash Can



Figure 6.2.42 Recycling Canisters



Figure 6.2.43 Trash Can

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Analysis

The buildings located in the City Center section of downtown Monett were built during the past 130 years and represent a range of architectural styles. These buildings, generally, create a cohesive downtown character. The majority of the buildings are two to three story masonry structures with businesses on the ground floor and offices, apartments, or storage on the upper floors.

Many of the buildings have metal cladding applied to the facades which does not match the character of the original architecture and the overall downtown. This cladding also prohibits a variety of uses that the upper levels could accommodate.

Most of the buildings have ground floor tenants, yet there are a few vacant storefronts. Additionally, there are few missing buildings in the City Center section of downtown. The combination of an “intact” street edge and few vacant storefronts helps to create a strong physical and perceptual sense of downtown.

Incongruence in building heights and materiality does weaken this sense of city center. The construction of one story structures, introduction of metal buildings and bricked up storefront windows in the downtown is unfortunate.



Figure 6.2.44 Windows that have been boarded



Figure 6.2.45 Uneven building heights

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Figure 6.2.46 Metal Cladding over original facades



Figure 6.2.47 Empty lower level of a building that weakens the streetscape

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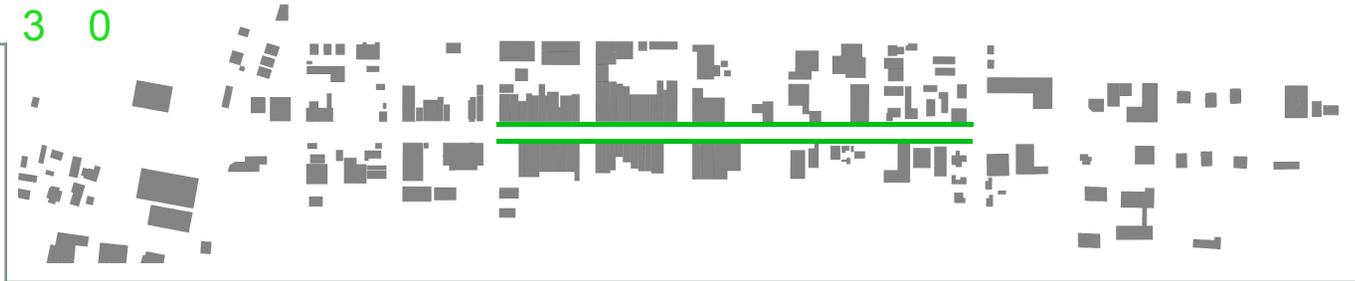
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Block F before



Block F after

Figure 6.2.48 Facade Study Before and After

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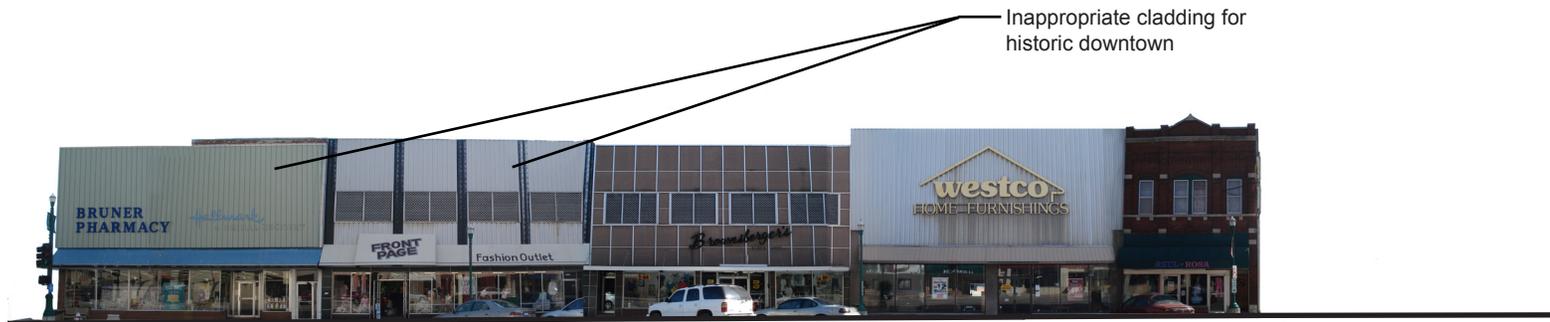
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Block G before



Block G after

Figure 6.2.49 Facade Study Before and After

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Facade Study



Block H before



Block H after

Figure 6.2.50 Facade Study Before and After

MONETT, MISSOURI

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Inappropriate building type for downtown area

Block I before



Potential location for the main entrance to the City Center Park

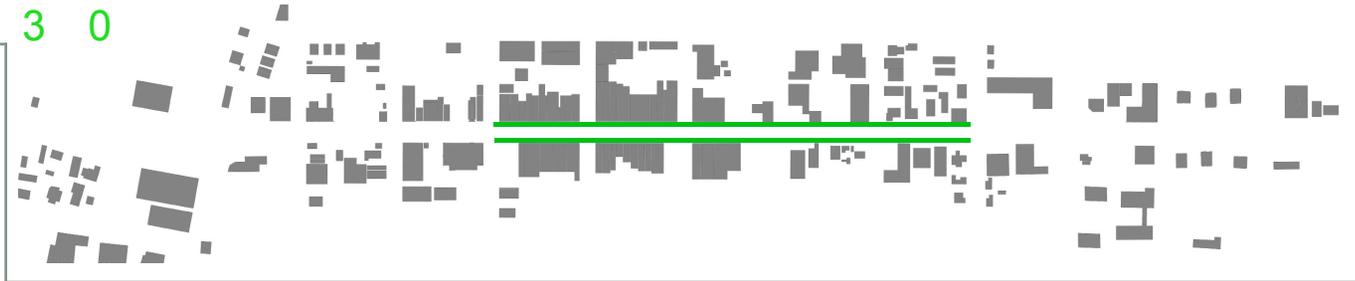
Block I after

Figure 6.2.51 Facade Study Before and After

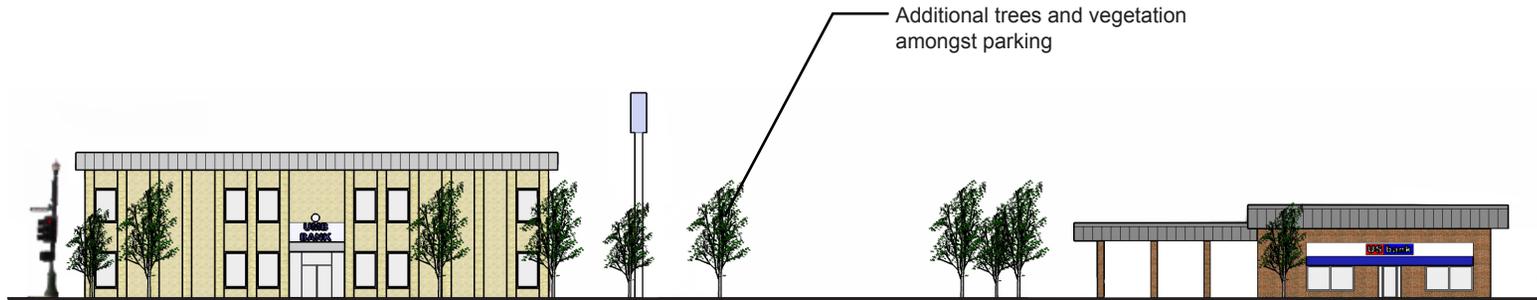
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Block J before



Block J after

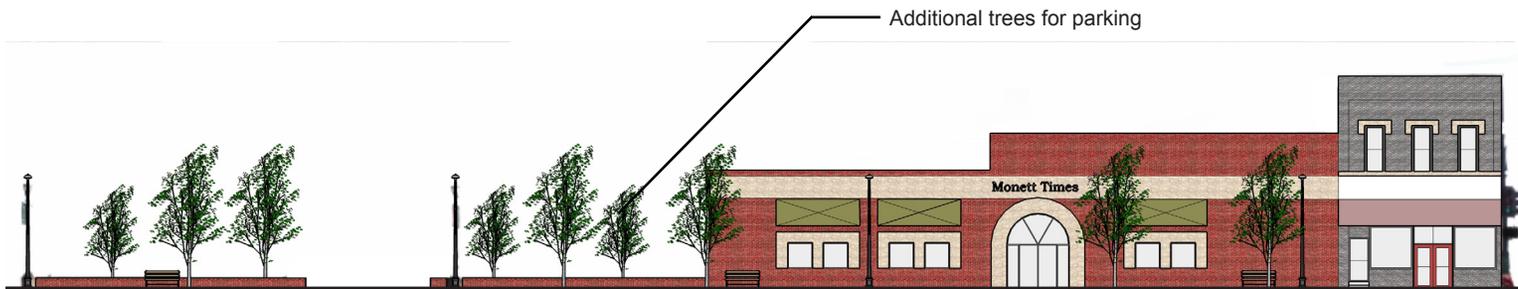
Figure 6.2.52 Facade Study Before and After

M O N E T T , M I S S O U R I

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Block K before



Block K after

Figure 6.2.53 Facade Study Before and After

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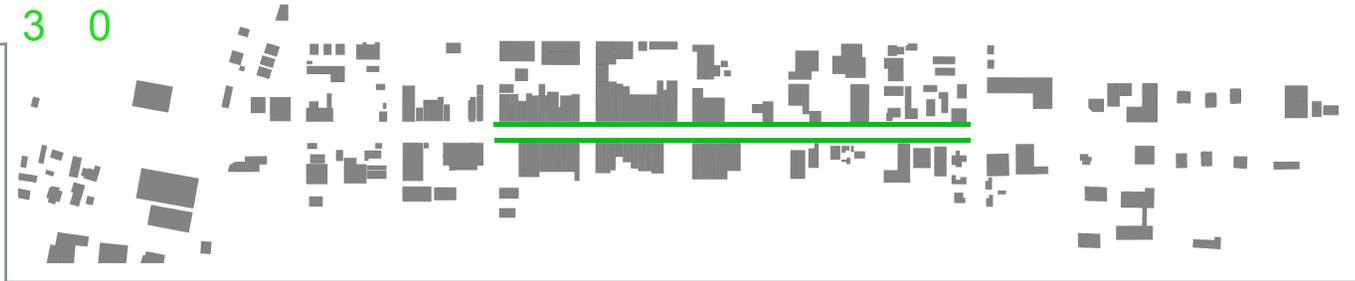
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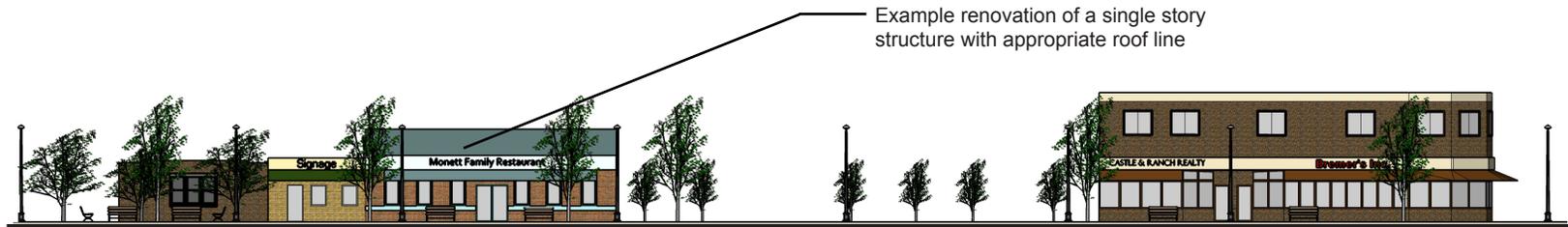
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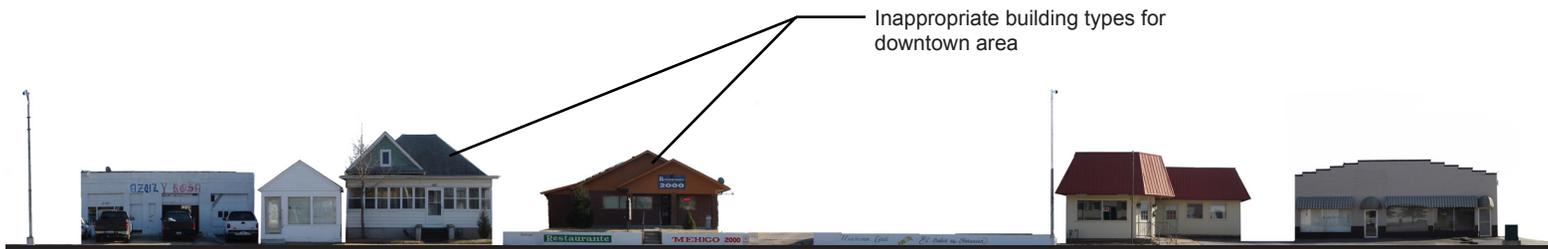
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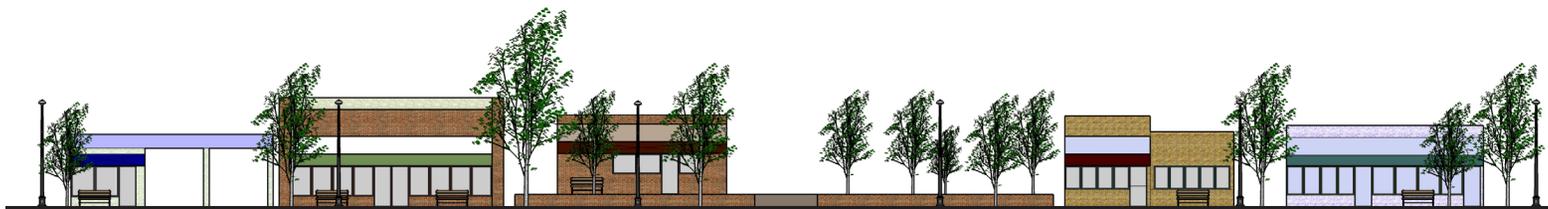
Block L after

Figure 6.2.54 Facade Study Before and After

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Block M before



Block M after

Figure 6.2.55 Facade Study Before and After

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Storefronts

The buildings in the downtown area should have large storefronts which directly connect with the sidewalk both visually and physically. The storefronts should unite the business with the sidewalk to create a seamless flow for pedestrians to transition from the streetscape into the stores. We recommend establishing new businesses in abandoned or empty buildings to open up the storefront and create a constant block of activity. All of the buildings in the downtown should be rejuvenated to allow for connectivity between the store and the streetscape.

Storefront Design

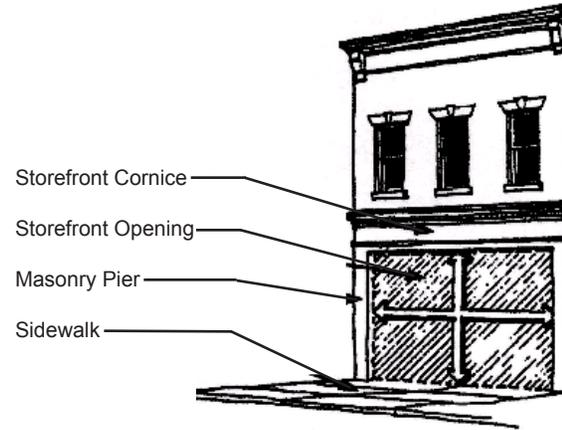


Figure 6.2.57 Storefront Components

Storefront Materials

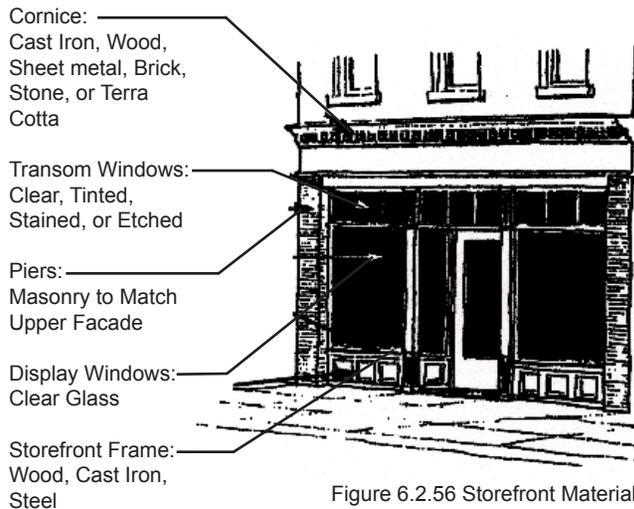


Figure 6.2.56 Storefront Materials

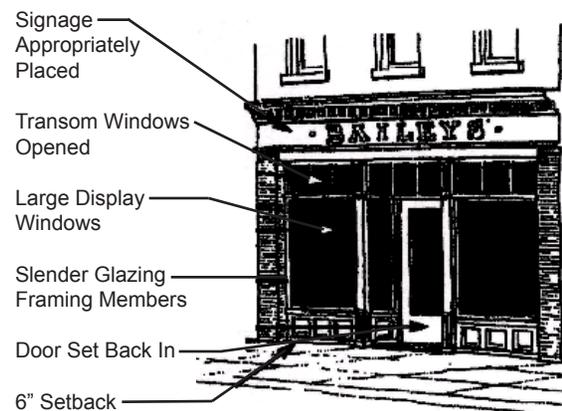


Figure 6.2.58 Storefront Components

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Awnings

Awnings are an asset to the facades of buildings as well as to the streetscape. Many awnings provide needed shade over the buildings storefront and reduce the building's heat gain which minimizes energy costs. Some awnings do not shade as well as others but still adds an aesthetic quality to the façade. All awnings help make the sidewalks a more intimate, human scale for pedestrians. By incorporating awnings to the facades, an overall cohesion would be created in the urban core area.



Figure 6.2.59 Metal awning



Figure 6.2.60 Canvas awning



Figure 6.2.61 Glass awning



Figure 6.2.62 Sleek awning



Figure 6.2.63 Metal awning

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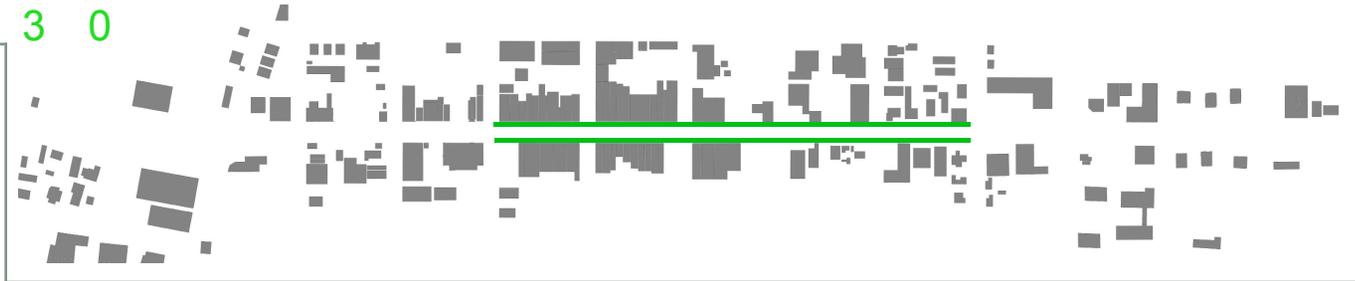
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Block N before



Block N after

Figure 6.2.64 Facade Study Before and After

M O N E T T , M I S S O U R I

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Block O before



Block O after

Figure 6.2.65 Facade Study Before and After

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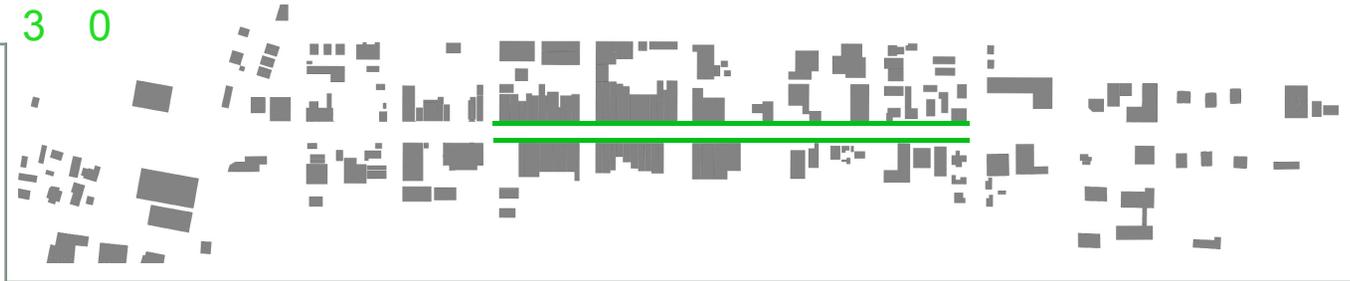
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Facade Study



Cleaning and Repair

Special attention should be made to restoration of the existing buildings. Care should be given in the analysis of whether façade materials should be restored or replaced depending on their current conditions. The most commonly used original materials in the downtown area are wood and brick. There are different techniques that can be used to return the various materials back to their original character, however all work should be completed by professionals because these techniques can do more harm to the brick or wood if completed improperly. Pressurized water and acid washings are the most common methods used for brick restoration. Acid washing uses chemicals to strip away at paint that has been applied to brick however it is an extremely difficult task. Sandblasting is an option but it should not be used on historic brick because it actually wears down the brick and removes any weak particles. Mortar replacement options should be carefully considered because new mortar compounds could ruin old brick. Wood should be carefully scraped of paint, primed, and then repainted finish the restoration processes.



Figure 6.2.66 Brick that needs to be cleaned

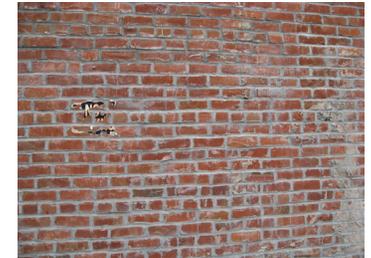


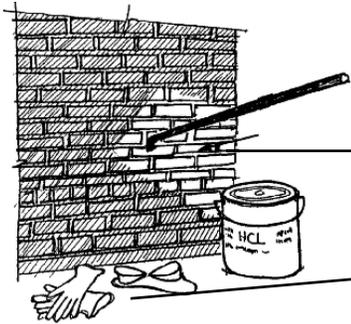
Figure 6.2.67 Worn brick



Figure 6.2.68 Brick that needs to be replaced

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Acid Cleaning

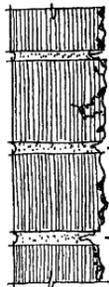


Do a "test patch" first to verify effectiveness

Acid cleaning requires equipment and safety precautions

Sandblasting

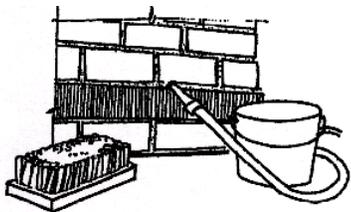
Even though effective in dirt and stain removal, damage occurs in both masonry and mortar joints



Pitted face, leaving softer surfaces exposed to weathering

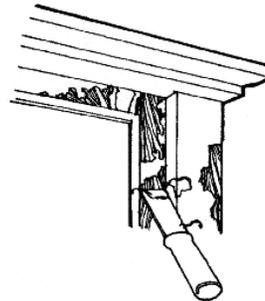
Mortar joints damaged

White Wash

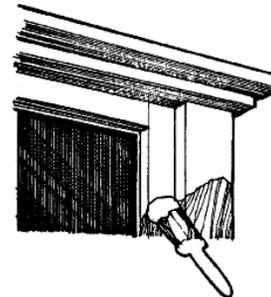


Use of a detergent and scrub brush (manual or electric) is the safest method, but also the most time consuming

Painting Wood



Scrape off old or loose paint, and use wood filler where wood is damaged. Replace only the piece of wood that is damaged rather than replacing the entire wood frame.



Prime and paint raw wood. Paint remainder of wood to match the entire frame.

Figure 6.2.69 Restoration Guidelines

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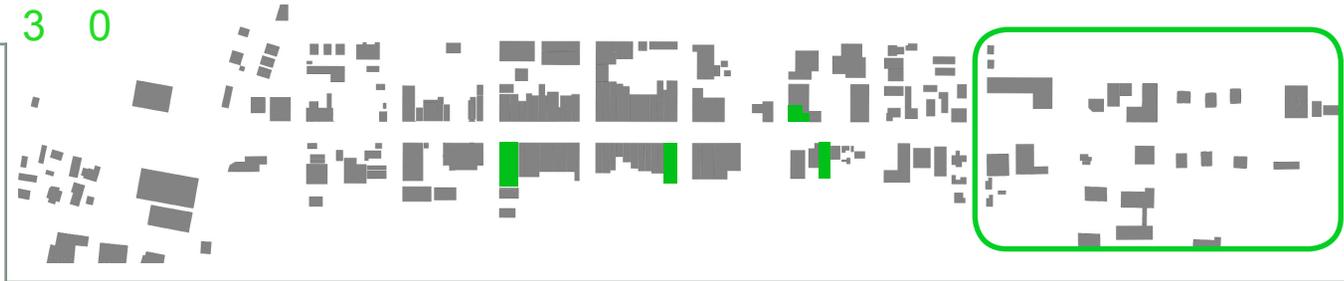
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Pocket Parks



Currently there is limited space in the downtown that is dedicated to green space. Pocket parks are small green spaces between buildings and along the street edge that can be used to soften the streetscape and allow for a more natural setting for relaxation and socialization within the City Center district. With the development of the new City Center Park, we recommend creating pocket parks that would allow for the park to filter between buildings up to Broadway. The pocket parks could also be used to link the the neighborhood north of Bond Street with the downtown and City Center Park. We also recommend that public restrooms be located within the pocket parks for additional relief for the users of the downtown area.



Figure 6.2.70 Pocket Park in New York that provides relief to the hard surfaces of the downtown



Figure 6.2.71 Proposed pocket park near new coffee shop and public restrooms on the corner of Broadway Street and 3rd Street.

GATEWAY EAST

Gateway East

The Gateway East district serves as the main entrance to the downtown from Kyler Street and extends west to Eighth Street. The Gateway East area has a larger prominence of residential buildings than the adjacent City Center district and the existing commercial buildings are less dense and tend to be freestanding. The mixing of commercial and residential buildings can lead to a dynamic juxtaposition of life throughout all times of the day. We propose that the Gateway East address the needs of residents, unite the dense neighborhood just north of Bond Street to the new City Center Park, and allow for the current commercial buildings to better relate to the rest of the downtown districts.



Figure 6.3.1 Gateway East Site Plan

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GATEWAY EAST

Gateway



Views

Currently, the intersection of Kyler Street and Broadway Street is overshadowed by the raised bridge that extends over the railway. We propose constructing a tall vertical landmark that can easily be seen and recognized by drivers as they come north across the bridge. We recommend that the vertical monument be a clock tower to emphasize the historic downtown. Digital signage could also be applied to the clock tower for advertisements of events that could occur within the downtown zone or the City Center Park.



Figure 6.3.2 Approach to Broadway Street from Kyler Street



Figure 6.3.3 View from Broadway

GATEWAY EAST

Socialization

The site of the gateway serves an important purpose because it begins to link the downtown to the neighborhood to the north, the park to the south, and even a more cultural diverse area of town to the east. In order to respond to the unique dynamic that converges at this one point, we recommend establishing areas of interaction. We suggest that adjacent to the clock tower, there be a green plaza area with benches to chat and socialize as well as open areas for group activities. This site also sits along Kyler Street, which the city of Monett as designated as their current greenway trail. This plaza then could also serve as an enjoyment stop along the trail for biking and walking.



Figure 6.3.4 Rendering of plaza space for proposed gateway

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Gateway



Accessibility

The tower and its adjacent construction should be accessible to encourage exploration of the site. We recommend creating a fountain that can be viewed from ground level or on an upper level that could be accessed by stairs or by an elevator within the clock tower itself. By creating different levels that can be accessed, the visitor of the site has a variety of ways to interact with the site which encourages longer usage.



Figure 6.3.5 Proposed Clock tower and elevated plaza space



Figure 6.3.6 Rendering of fountain area

GATEWAY EAST

Streetscape

Parking

It is recommended that the parking in the Gateway East section of downtown have the same feel as the other sections of the street and streetscapes. However, parking lots will replace parallel parking from 9th Street east to Kyler to focus on the family feel of the Gateway East rather than the business focus of the City Center. There will be a few new parking lots added to handle the increase of traffic at the Development Center and the east entrance into the City Center Park. In addition to the lot parking there will be private parking for residents in the alleyway just north of Broadway.



Parking Spaces per Block	Block P	Block Q	Block R	Block S	Block T	Block U	Block V	Block W	TOTAL
Existing On Street	47	27	4	0	6	6	8	0	98
Existing Off Street	0	0	0	0	0	0	0	0	0
Proposed On Street	41	27	12	35	18	23	27	0	193
Proposed Off Street	10	0	10	8	0	0	0	0	28
Difference	+4	0	+18	+43	+12	+17	+19	0	+113

Figure 6.3.7 Parking

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Streetscape



Lighting

The lighting should enhance the character of the district and encourage visitors to experience the entire downtown area during the day and evening. The lighting should enhance and invite pedestrians to explore the Gateway East along with the new activities.

The new pathways; sidewalks, pocket parks, and benches, should all have adequate lighting in the nighttime. The placement and type of light that are installed will provide a safe environment for vehicles and pedestrians. Many communities have zoning regulations which require a minimum amount of illumination for these spaces.



Figure 6.3.8 Lighting with Planters



Figure 6.3.8 Lighting character



Figure 6.3.9 Festive lighting options

GATEWAY EAST

Vegetation

Along Broadway and the new pathways there is a need for shading along with green spaces that provide for water saturation and rest area. The vegetation along the road can also provide a buffer along the road while still continuing the “street edge” in the downtown area. Creating different variations in vegetations will provide more of a dynamic environment for the pedestrians.

This vegetation should also extend the downtown area along all 13 blocks and yet still have a unique feel for the Gateway East. A feel of more open and softer feel which will connect the area with other residential areas within the city.



Figure 6.3.11 Vegetation used to maintain the street edge when a parking lot extends between the the sidewalk and the building

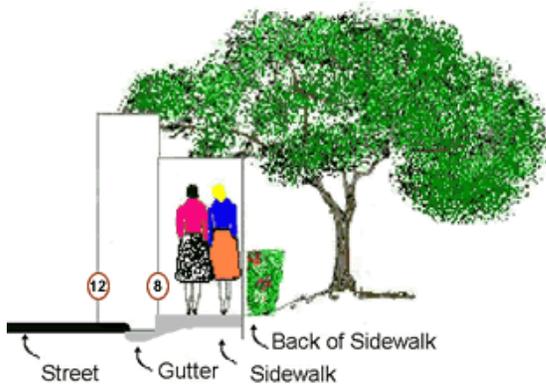


Figure 6.3.10 Typical streetside path



Figure 6.3.12 Vegetation along a pathway

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Streetscape



Signage

This is an element that can add to the overall flow of the downtown area while offering information about businesses and other general facts to locals and visitors. As with the lightpost, there are many different types of signs that could be used to reflect the feel of that area.



Figure 6.3.13 Proper signage

The placement of these new signs are equally important for reasons of visibility. Some signs can be placed in store fronts, building facades and lightposts.



Figure 6.3.14 Informational signage



Figure 6.3.15 Proper placement of storefront signage

GATEWAY EAST

The Facade

Strip Mall

Currently there is a strip mall on the north side of Broadway Street between Eight and Ninth Streets. This site has excellent potential for increased business opportunities. There currently exists a large parking lot that is not being utilized to its fullest potential.

The façade of the strip mall should be renovated in brick to match the historic downtown characteristic and the street edge should be maintained without losing the existing parking. This could be achieved by creating a barrier between the parking lot and sidewalk that is full of vegetation and seating. This would also promote the social interaction between with the east district and city center.



Figure 6.3.16 Existing Condition of the Strip Mall



Figure 6.3.17 Rendering of newly designed lot



Figure 6.3.18 Aerial view of the Strip Mall development

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Pocket Parks



Pocket parks are open spaces within an urban area that encourage social interaction. The green spaces in the Gateway East area will become pocket parks that will provide a network of connections from Bond Street to the City Center Park. These pocket parks will be located throughout the Gateway District. Our design proposal creates a number of seating area to reinforce the street edge. Trees and other types of vegetation should be placed to help soften the hard existing surfaces.



Figure 6.3.20 Seating in a pocket park



Figure 6.3.19 Pocket park courtyard



Figure 6.3.21 Place for social interaction

GATEWAY EAST

Development Center

We recommend creating a social center on the south side of Broadway Street between Eight and Ninth Streets. This center should work in conjunction with the senior citizens and the YMCA in providing outreach to the community in times of need. The center would be a location where many different activities could be held, such as afterschool programs, a cultural theatre, a multi-purpose room, as well as a place for families and/or adults to find assistance.

With the City Center Park being such a prominent aspect of the new design for the downtown area, we suggest a connection between the park and the center. The proposed design of the building spans over the existing canal and directly links to the park. The center would serve as a pivotal transition between the park and the east district of the downtown.



Figure 6.3.22 Existing site the Foundry



Figure 6.3.23 Rendering of activities of the Development Center



Figure 6.3.24 Front Facade of the Development Center

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We propose creating a City Center Park which would extend between Highway 37 and Kyler Street and between Broadway Street and the rail line to the south. This location provides connections with the downtown as well as the northern and southern sections of Monett through greenway trails. Some portions of the park should even extend to pocket parks to serve as entrances and unite the downtown with the new centralized park. This linear park should serve as a major hub for social activities. We recommend the park include amenities such as a farmer’s market, events pavilion, playground, spray ground, amphitheater, group and family pavilions, a water silo recreation area, community garden, bike and pedestrian trails and a connection to the proposed community center. Both citizens and visitors of all ages and ethnic backgrounds would be able to come together and celebrate the rich culture of Monett.

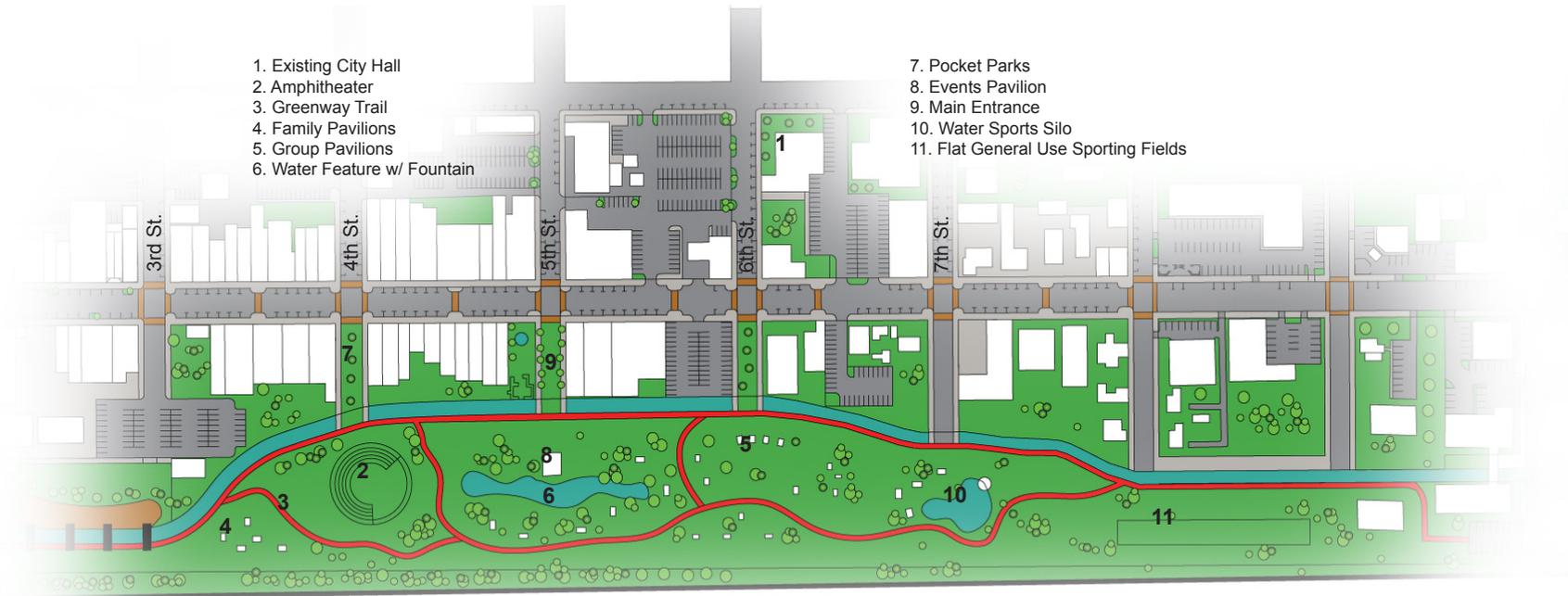


Figure 6.4 Master Plan of City Center Park

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Figure 6.4.1 View of the envisioned 5th St. entrance to the park



Figure 6.4.2 Example of fountain and green space



Figure 6.4.3 Example of park entrance

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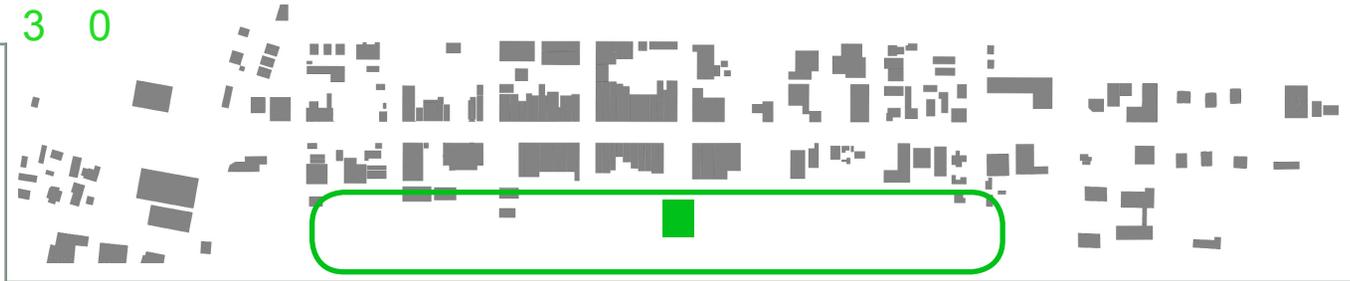
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Channel Renovation



In order to transition between the downtown and the park, the existing channel must be crossed. Currently, the channel lacks an aesthetic that is pleasing to the eye and the concrete and stone only promotes rushing water. With this in mind, we recommend the current channelized water system be left in existence, yet modified to efficiently help slow the water flow and allow for a more natural appearance. We recommend removing much of the concrete and stone and plant native vegetation in its place. This anchors the channel from erosion as well as promotes natural life cycles.



Figure 6.4.2 View of the beautification of existing channel and envisioned greenway trail



Figure 6.4.4
Prairie Blazing Star



Figure 6.4.5 American Smoketree



Figure 6.4.3
Little Bluestem



Figure 6.4.6 Prairie Dropseed



Figure 6.4.7 Aromatic Aster

CITY CENTER PARK

Events Pavilion

We recommend establishing an events pavilion that would serve the community of Monett in multiple capacities and provide shelter for a variety of activities. Different groups such as orchestras, high school organizations, local and regional musicians, and city officials are just a few that would be able to rent the pavilion for special meetings or performances. Ultimately, the pavilion could be used to establish revenue for the city and promote positive social interaction between community members.

We propose creating a small water feature just south of the pavilion that guests could overlook. Fountains should be placed within the water feature so as to provide visual and audible experiences as well as keep the water moving to prevent algae buildup. This water feature would also allow animals such as ducks and geese to inhabit this natural space.



Figure 6.4.8 Events pavilion



Figure 6.4.9 Events pavilion at night

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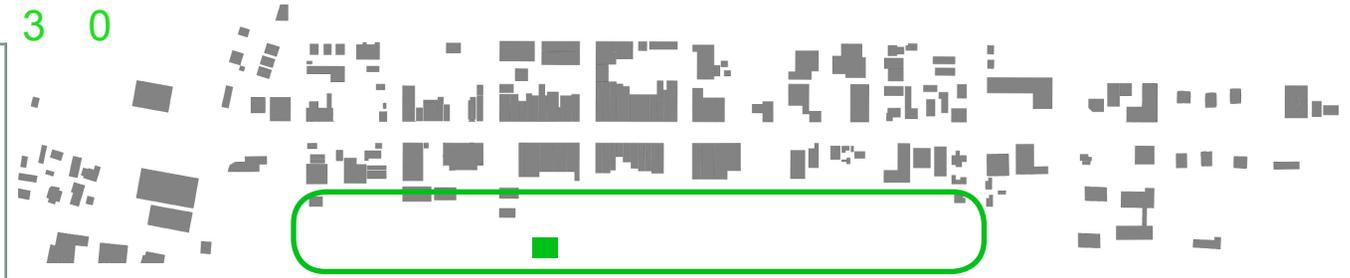
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Amphitheater



We recommend constructing a natural earth bermed amphitheater that will serve the community as an outdoor public meeting space as well as a space for outdoor concerts and events. The seating space for the amphitheater should be constructed from concrete in order to be able to withstand a flood event and to prevent damage during continuous use. Seating should accommodate at least 400 people. A “central stage” area should be at the base of the amphitheater and could double as a sprayground for kids.



Figure 6.4.11 Example of amphitheater



Figure 6.4.12 View of amphitheater from greenway trail area



Figure 6.4.13 View of amphitheater

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Greenway Trails

We recommend establishing a greenway trail that runs through the linear park and connects South Park to North Park. Bike and pedestrian traffic would be able to flow easily along this new section of the trail which we recommend as being a ten foot wide path out of porous concrete. This would decrease the amount of water runoff from the trail as well as connect important features and amenities within the city. Along the trail, there should be seating and way finding signage. The signage should be placed in such a manner that points users towards certain businesses and built features within the park area as well as along Broadway Street. These signs should serve as key points of arrivals within the City Center Park greenway network. These signs should be designed and well crafted in order to create a cohesive appearance within the park.



Figure 6.4.16 Greenway Concept Sketch Precedent



Figure 6.4.17 Installation of vegetation along greenway



Figure 6.4.14 Porous pavement

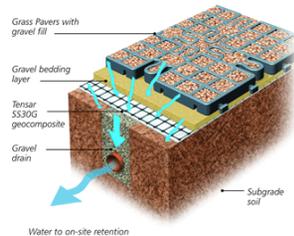


Figure 6.4.15 Porous pavement construction



Figure 6.4.18 Urban greenway

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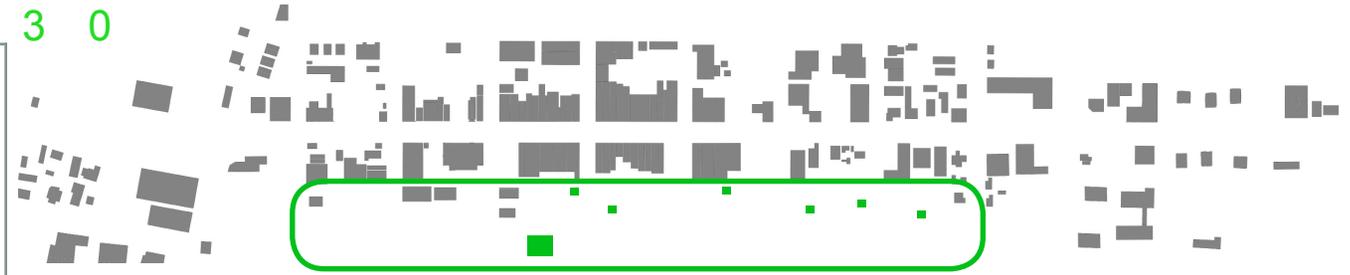
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Family and Group Pavilions



Throughout the park we recommend constructing, with similar materials and form as the event pavilion a few smaller sized pavilions, which would be utilized by families and smaller groups throughout the park. These pavilions should be placed in strategic locations in the City Center Park along the greenway trail network.



Figure 6.4.19 View of envisioned pavilion



Figure 6.4.20 Couples picnic in the park



Figure 6.4.21 Walk in the park



Figure 6.4.22 Family picnic in the park

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Spray Ground

We propose implementing a spray ground to create a safe place for children to play that would be a fun and sustainable feature. Unlike water parks, these spray grounds can serve the community from early spring to late fall and should be constructed of concrete that is sloping towards the middle of the structure. There should be water elements including buckets and spray devices that will allow water to flow out onto the pavement. Jets located within the concrete would spray upwards like a fountain creating enjoyable activities for everyone. The water from this splash pad would be funneled into an underground filtering and pump system thus allowing the same water to be re-used over and over again.

This feature is an alternative to a neighborhood swimming pool and relies on either manual or self timed controls for the output of water. Kids can be in control of certain elements within the pad and can increase or decrease the flow as they wish. The risk of drowning is diminished with spray grounds because of their lack of water collection. Additionally, these splash pads allow those with handicaps to enjoy the same pleasures as people that are mobile.



Figure 6.4.23 Sprayground



Figure 6.4.24 Recycled Water



Figure 6.4.25 Fun For All Ages

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Water Sports Area



We recommend that something be done beside and in conjunction with the abandoned grain silo off of Front Street. and 7th Street. We propose creating an interactive amenity with a water feature to host a variety of sports and leisure activities. Canoeing, kayaking, fishing, and swimming could occur within the water feature. The silo exterior should be utilized for rock climbing and the interior could store equipment. The vertical silo should also serve as an icon to the City Center Park.



Figure 6.4.26 Kayaking



Figure 6.4.27 View of water sports area



Figure 6.4.28 Fishing

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Sections through park

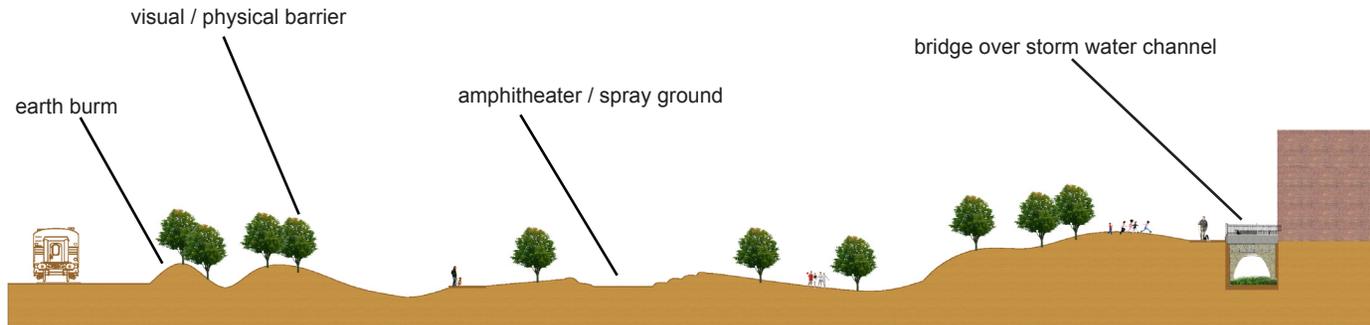


Figure 6.4.29 Section through park showing amphitheater and terrain

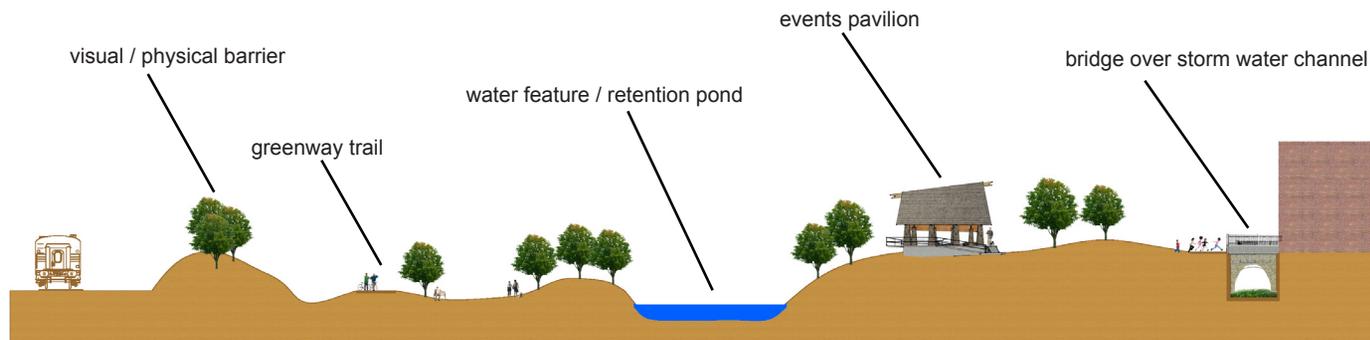


Figure 6.4.30 Section through park showing water feature and events pavilion

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